

Design Guidelines

Denver's Designated Parkways and Boulevards



City and County of Denver

November 2005

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Speer Boulevard, circa 1904-1920
courtesy Western History Collection, Denver Public Library

[Foreword]

June 30, 2004

To all Denver residents:

When Denver was transformed from a frontier town into a city, a tight grid of streets was laid over the high prairie. That elaborate street system became one of the most significant civic improvements in Denver's history. The grid not only organizes Denver into neighborhoods, it a hive of activity. The streets are moving corridors, carrying automobiles, bicycles, pedestrians, and utilities above ground; and water, utilities, telecommunications, and waste below ground. The streets, with their public right-of-ways on either side, are the largest piece of publicly owned land in the city.

Their role as civic space is significant. In addition to their functional duties, the streets, with their tree-lined sidewalks in the older sections of Denver and more contemporary landscaping elsewhere, also create the green oasis, visual beauty, and character that define Denver. And within this system, a framework of special streets-Denver's designated parkways and boulevards elevate The Street to an even higher standard of urban design. They are both street and parkland and are the ribbon that ties our historic parks and neighborhoods together. Their importance is recognized by their listing on the National Register of Historic Places.

This *Assessment of Denver's Historic Parkways and Boulevards*, including both *Design Guidelines* and *Management Recommendations*, is critical to protect and manage these valuable streets for future generations. They acknowledge that, even with their core functional role, Denver's parkways and boulevards warrant a special level of care, of standards, of protection. Otherwise, the physical pieces, which together create these special corridors of park land, can be whittled away by change and neglect until they are lost. Colorado Boulevard is a striking example of the ruination of a parkway from the lack of protection and insensitive management.

Our three departments, Public Works, Parks and Recreation, and Community Planning and Development have varying degrees of responsibility for this rich blend of street and park. Because of that complexity, we are committed to working together, to supporting and implementing these important recommendations and guidelines. We want to ensure that future generations will enjoy these pleasure drives, these public spaces.

[Acknowledgements]

JOHN HICKENLOOPER

Mayor

Kim Bailey

Manager of Denver Parks and Recreation Department

Bill Vidal

Manager of Public Works Department

Peter Park

Director, Community and Planning Development

Parks and Recreation Department

Susan Baird

Helen Kuykendall

Mark Upshaw

Maureen Van Norden

Denver Public Works Department

Mark Najarian

Rob Duncanson

Community Planning Department

Ellen Ittleson

Denver Landmark Preservation Commission

Chris Murata

Colorado Historical Society's State Historical Fund

Jim Joy

Consultants

Mundus Bishop Design, Inc.

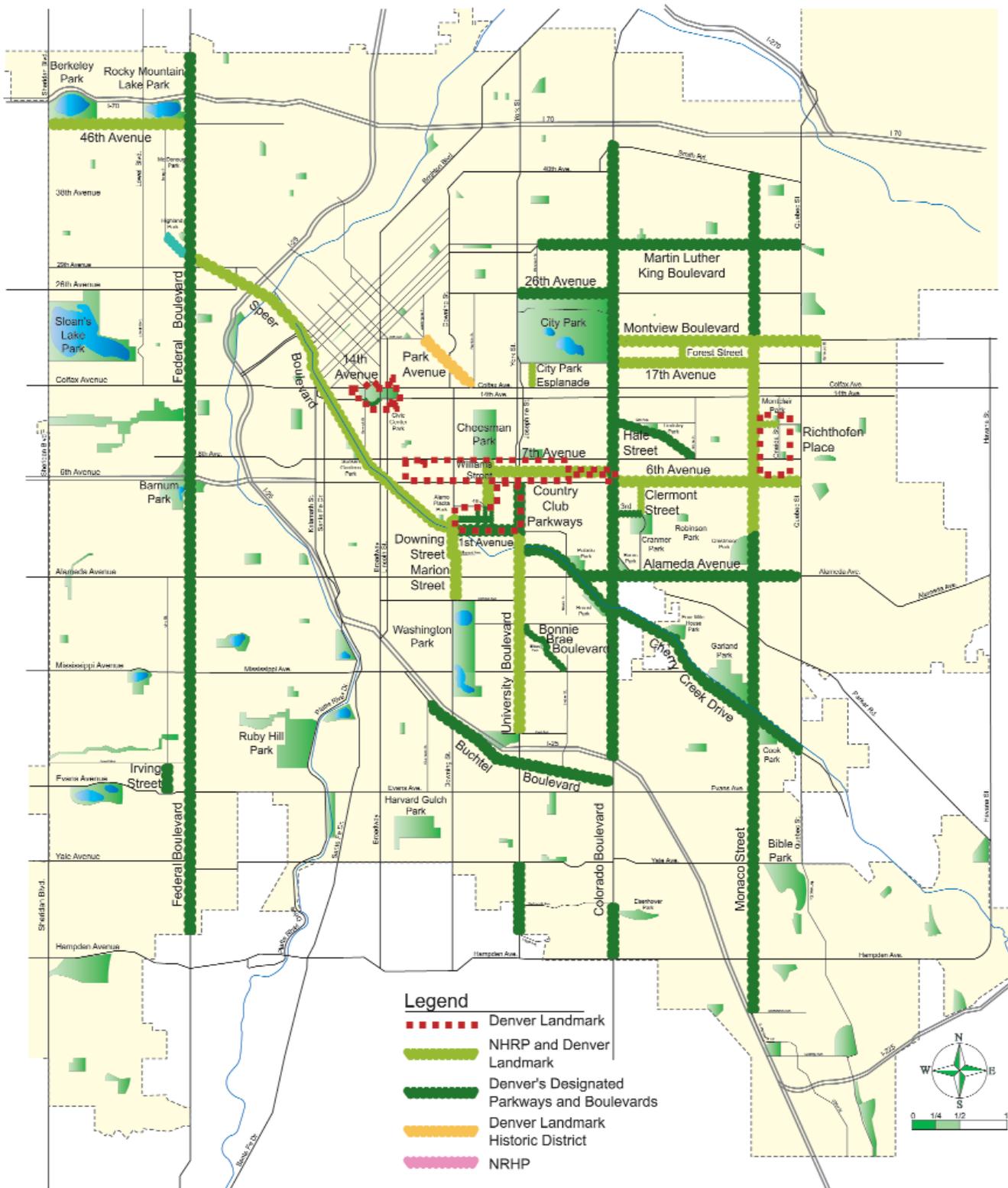
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[Table of Contents]

Section 1 - Introduction 1
 General 2
 Review of Modifications 4
 Definition of Terms 6
 Section 2 - Understanding Denver’s Parkways and Boulevards 8
 Context 8
 Historical Development 8
 Section 3 - General Design Guidelines (General to Specific) 14
 A.1 Preservation of Character 14
 A.2 Preservation of Original Features 15
 A.3 Street Cross Sections and Topography 16
 A.4 Tree Lawns 17
 A.5 Sidewalks 18
 A.6 Building Setbacks and Siting Buildings and Structures 20
 A.7 Monuments and Gateways 21
 A.8 Signs 22
 A.9 Vegetation 24
 A.10 Fences and Structures 26
 A.11 Pedestrian Lighting 27
 A.12 Street Lighting 28
 A.13 Driveways, Alleys and Parking 29
 A.14 Bus Stops 30
 A.15 Utility Boxes, Mailboxes and Underground Utilities 31

Appendix
 A. Parkway and Boulevard Ordinance
 B. Parkway and Boulevard Building Setbacks and Sign Setbacks
 C. Denver Landmark Historic Districts with Parkways and Boulevards

Designated Parkways and Boulevards



Denver's Designated Parkways and Boulevards

Section 1. Introduction

General

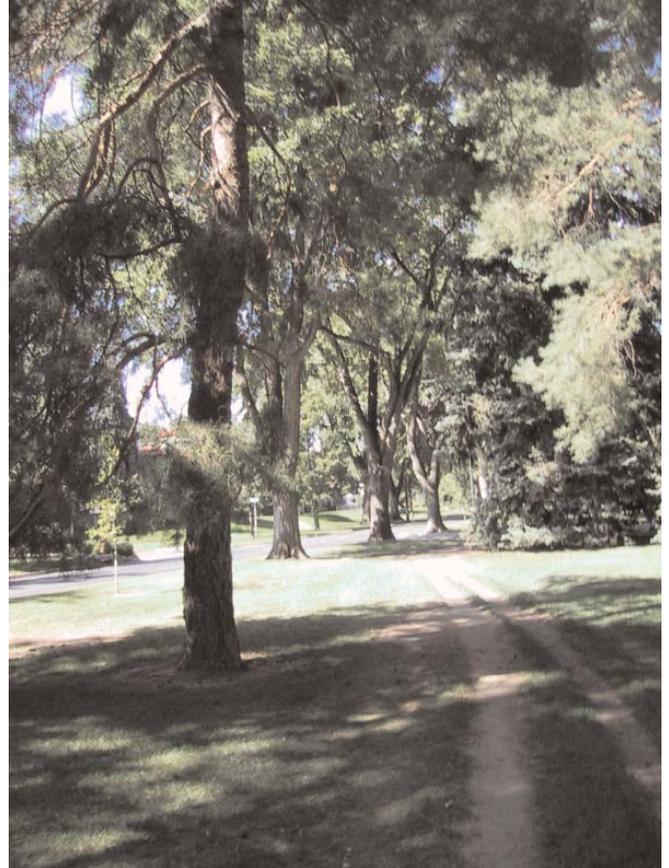
Denver’s rich legacy of 35 Designated Parkways and Boulevards is the framework upon which the early city developed. Carefully designed as grand tree-lined avenues, these wide landscaped streets with park-like settings are special city streets that share certain unifying features but are individually distinct. Beginning with the early 1870s construction of Federal Boulevard and Park Avenue, the city’s grid of streets has been continually enhanced by broad tree-lined avenues that connect the city’s parks, civic spaces and residential neighborhoods.

Located throughout the city, the Designated Parkways and Boulevards are 35 streets that are officially protected through a series of city ordinances that were enacted between 1950 and 1977. The Parkway and Boulevard Ordinance names 35 streets as designated parkways and boulevards and sets regulations related to the public right-of-way and the adjacent property within the building and sign setback. For example, the ordinance provides for “setback regulation of structures,” requires that buildings front on the parkway or boulevard, and mandates that advertising is not allowed.

In addition to designation by the Parkway and Boulevard Ordinance, twenty-one of the parkways and boulevards are historically significant and are designated within one or more Denver Landmark Historic Districts and are listed on the National Register of Historic Places.

The purpose of these Design Guidelines is to define the features that characterize each of the 35 Designated Parkways and Boulevards, and to provide guidance in the design and review of any proposed alterations or changes to ensure that the character of each parkway and boulevard and the system is preserved while contemporary needs are met.

These Design Guidelines are intended to be used by all city departments, developers, private property owners, residents, merchants and designers in creating and reviewing proposed changes. These guidelines apply to all proposed alterations or changes including those proposed by adjacent property owners or by the City & County of Denver.



East 6th Avenue Parkway

| Denver's Designated Parkways and Boulevards | |
|---|--|
| East Alameda Avenue | Steele Street to Quebec Street |
| Bonnie Brae Boulevard | Mississippi Avenue to Exposition |
| Buchtel Boulevard | Clarkson Street to Colorado Blvd |
| Cherry Creek Drive | University Blvd to Quebec Street |
| City Park Esplanade** | Colfax Avenue to East 17th Avenue |
| Clermont Street Parkway*** | East 3rd Avenue to East 6th Avenue |
| Colorado Boulevard | Hampden Ave to Dartmouth Ave and I-25 to 44th Avenue |
| Country Club Parkways*** | |
| Franklin Street | East 1st Avenue to East 4th Avenue |
| Gilpin Street | East 1st Avenue to East 4th Avenue |
| High Street | East 1st Avenue to East 4th Avenue |
| East 3rd Avenue | Gilpin Street to Williams Street |
| East 4th Avenue | Gilpin Street to Williams Street |
| Downing Street Parkway*** | E. Bayaud Avenue to E. 3rd Ave |
| Federal Boulevard | south city limit to 25th Avenue and 26th Ave to north city limit |
| Forest Street Parkway*** | E. 17th Ave to Montview Blvd |
| Hale Parkway | 12th Ave/Colo to 8th Ave/Grape |
| South Irving Street Parkway | Evans Ave to W. Jewell Avenue |
| South Marion Street Parkway***Downing to E Virginia Ave | |
| Martin Luther King Jr. Blvd. | Elizabeth Street to Quebec Street |
| Monaco Street Parkway*** | Quincy Ave to 40th Ave/Smith Road |
| Montview Boulevard*** | Colorado Blvd to Syracuse Street |
| Park Avenue ** | Colfax Ave to 20th St/Washington St |
| Richthofen Place Parkway | Monaco Parkway to Oneida Street |
| Speer Boulevard*** | Federal Boulevard to Downing Street |
| University Boulevard*** | Floyd to Yale & Iowa St to 6th Ave |
| Williams Street Parkway*** | East 4th Avenue to East 8th Avenue |
| East 1st Avenue Parkway*** | Downing St to University Boulevard |
| East 3rd Avenue Parkway | Colorado Blvd to Clermont Parkway |
| East 6th Avenue Parkway*** | Colorado Boulevard to Quebec St |
| East 7th Avenue Parkway*** | Williams St to Colorado Boulevard |
| West 14th Avenue Parkway*** | Bannock Street to Broadway |
| East 17th Avenue Parkway*** | Colo Blvd to Monaco Street Parkway |
| East 26th Avenue Parkway | York Street to Colorado Boulevard |
| West 46th Avenue Parkway*** | Sheridan Blvd to Federal Boulevard |
| * National Register of Historic Places | |
| ** Denver Landmark Historic District | |
| *** Both | |

Review of Modifications

The Parkway and Boulevard Ordinance sets regulations and designates review authority of any proposed alterations or changes to the Manager of the Denver Parks and Recreation Department. Any alteration or modification within the public right-of-way or the parkway building and sign setback must be reviewed and approved by the Manager of the Denver Parks and Recreation Department.

The 35 designated parkways and boulevards are also functioning city streets and as such are managed by several departments within the Denver Public Works Department. Depending on the nature of the proposed alteration or modification, additional review and approval by the Public Works Department, Wastewater Management and Community Planning and Development may be necessary.

For the twenty-one parkways and boulevards that are also designated as part of one or more Denver Landmark Historic Districts, an additional review by the Denver Landmark Preservation Commission is required. Designation as a Denver Landmark Historic District recognizes the contribution that these parkways and boulevards have made to Denver's historical development and to its architectural character. It also provides protection in the form of design review and from demolition. As for all Denver Landmarks, any exterior alteration must be reviewed and approved by the Denver Landmark Preservation Commission prior to construction.

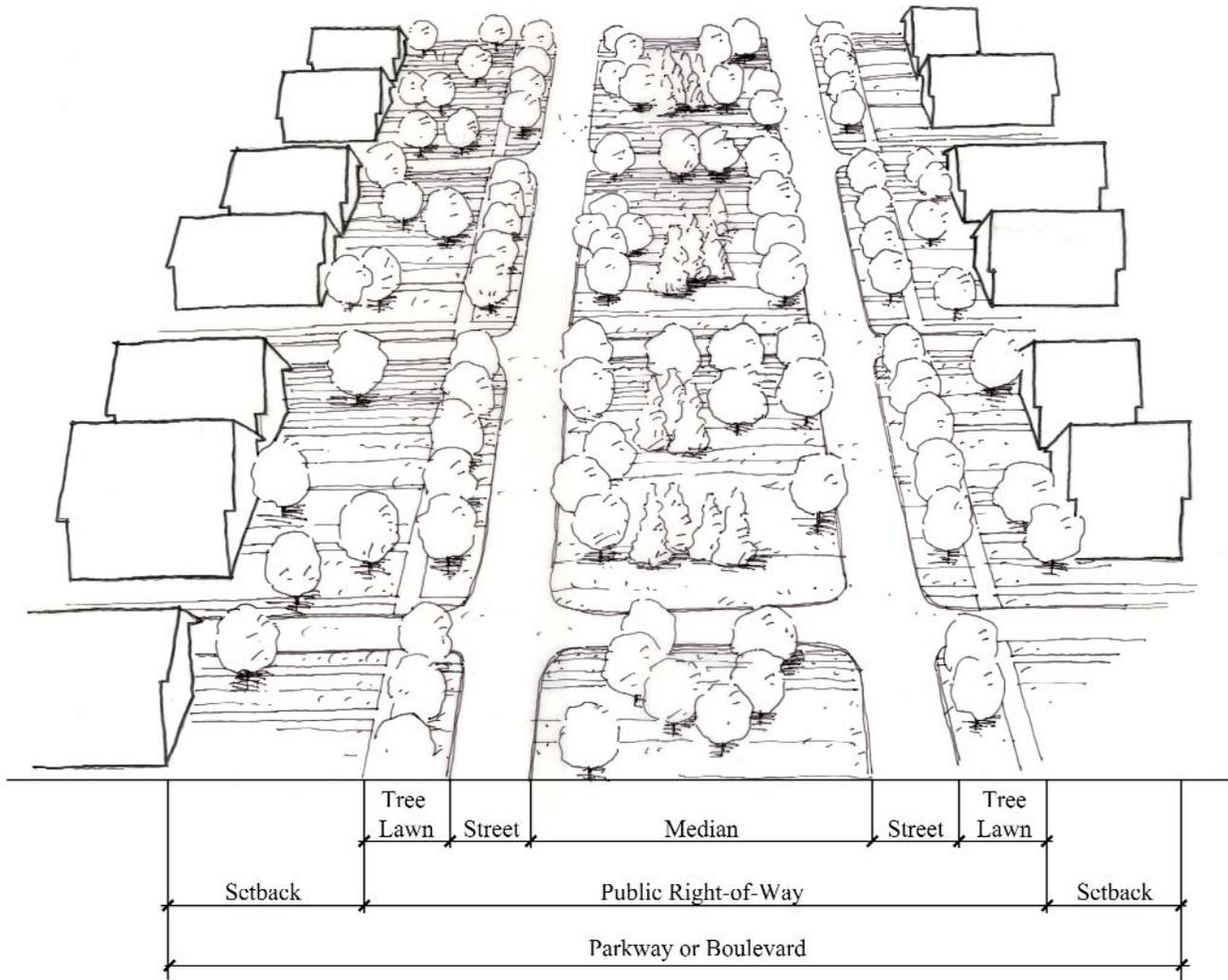
These *Designated Parkway and Boulevards Design Guidelines* are the primary set of guidelines for any alteration or modification to the parkways and boulevards designated by the Parkway and Boulevard Ordinance. These *Design Guidelines* work in tandem with the Parkway and Boulevard Ordinance, and provide guidance to private property owners and city agencies in the design, construction and maintenance activities within the public right-of-way and parkway building and sign setback. These *Design Guidelines* focus on the Designated Parkway and Boulevards themselves and are in no way meant to contradict the guidelines of the Denver Landmark Commission. If a discrepancy exists between these *Design Guidelines* and others the more stringent guideline shall apply.



*East 17th Avenue Parkway, circa 1910-1920
courtesy Western History Collection, Denver Public Library*

Review of Modifications

Alterations or changes within the Denver Landmark Historic Districts are subject to the *Secretary of Interior Standards for the Treatment of Historic Properties with "Guidelines for Cultural Landscapes," "Design Guidelines for Landmark Structures and Districts"* and for those parkways and boulevards in the Country Club area, the *Design Guidelines for the Country Club Historic District* apply.



Definition of Terms

Parkway and Boulevard: One of 35 streets that are designated as a parkway or boulevard by City ordinance that includes: Public right-of-way, Parkway building setback.

Right-of-Way: Public land that includes the street, the tree lawn and the sidewalk.

Parkway and Boulevard Ordinance: A City and County of Denver Ordinance that names 35 streets as parkways and boulevards.

Parkway Building Setback: The distance from the right-of-way that structures such can be located along a parkway or boulevard. Setback distances are established by the Parkway and Boulevard Ordinance to create a visually open green space that extends the park-like character to the building edge.

Parkway Sign Setback: The distance from the right-of-way that signs such as commercial signs shall be located along a parkway or boulevard. Setbacks are established by the Parkway and Boulevard Ordinance to ensure that the visual open character of the parkway or boulevard is protected.

Tree Lawn: The area between the sidewalk and the curb that is a character-defining features of each parkway or boulevard. Tree lawns are typically wide to create a distinctive park-like setting and are often planted with street trees to create a continuous canopy. Although they are within the city's right-of-way, tree lawns are maintained by the adjacent property owner.

Median: A broad, landscaped island that is located between two lanes of travel. Parkway and boulevard medians are traditionally very wide and are usually landscaped with trees and shrubs. Not every parkway or boulevard has a median.

Private Yard: The private yard is the space within the building setback, located between the right-of-way and the building. The yard is an important component of a parkway or boulevard, providing a consistent edge and a continuous open space on each side of the street

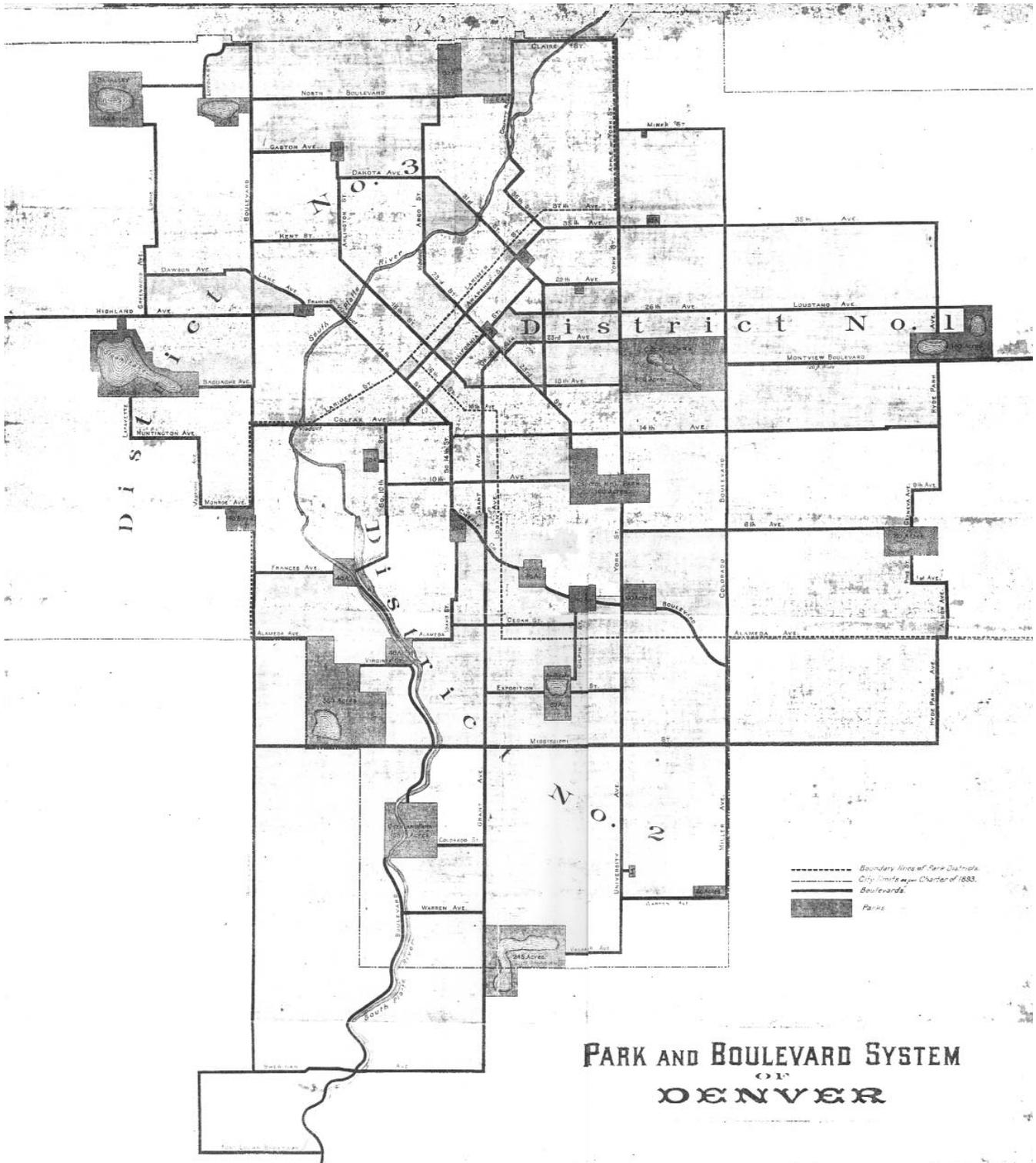


Irving Street Parkway



Forest Street Boulevard

Designated Parkways and Boulevards



Dr. John Evans' vision for an inter-connected parks and parkway system is evident in the plan he created with Edward Rollandet 1894. Courtesy Western History Collection, Denver Public Library

Section 2. Understanding Denver's Designated Parkways and Boulevards

Context

Denver's 35 Designated Parkways and Boulevards are a framework of interconnected park-like streets that knit the city into a cohesive whole, linking neighborhoods, parks and civic spaces while providing an enriching experience. The parkways and boulevards were envisioned as key civic connections from the earliest days of Denver's city planning, beginning with an 1894 plan for a park and parkway system that was proposed by Territorial Governor John Evans (as drafted by Edward Rollandet) and cumulating with the *1909 Adopted Plan* that was adopted by the city and is the framework for the system we know today.

With the construction of Park Avenue in 1874, the parkway and boulevard system began to define the character of Denver, particularly the eastern portion of the city. Over the course of 100 years, 34 more streets would become part of this system. Developed in three distinct periods with years of intense construction countered by years of little growth, the parkways and boulevards were built through the efforts of many locally and nationally known landscape architects, planners, engineers, artists and architects. Today, this system retains the basic characteristics of its original design and construction and continues to provide an aesthetic framework for Denver.

Historical Development

A Vision - 1874 to 1904

The desire to create places of enjoyment and respite are evident in city plans from as early as 1859 (the Surveyor's City Block Plan) when land was set aside for Curtis, now Mestizo Curtis and Lincoln parks. City beautification began in 1871 when the (Federal) Boulevard was laid out as a tree-lined street. By 1874, the Park Avenue Addition was platted through the collaborative efforts of its major property owners, including Dr. John Evans, Colorado's second territorial governor, to be ". . .the most sightly and beautiful ground in the city laid out in the most elegant manner with Wide Avenues, Numerous Parks, and Deep Lots." By 1893 it was planted as a tree-lined boulevard.



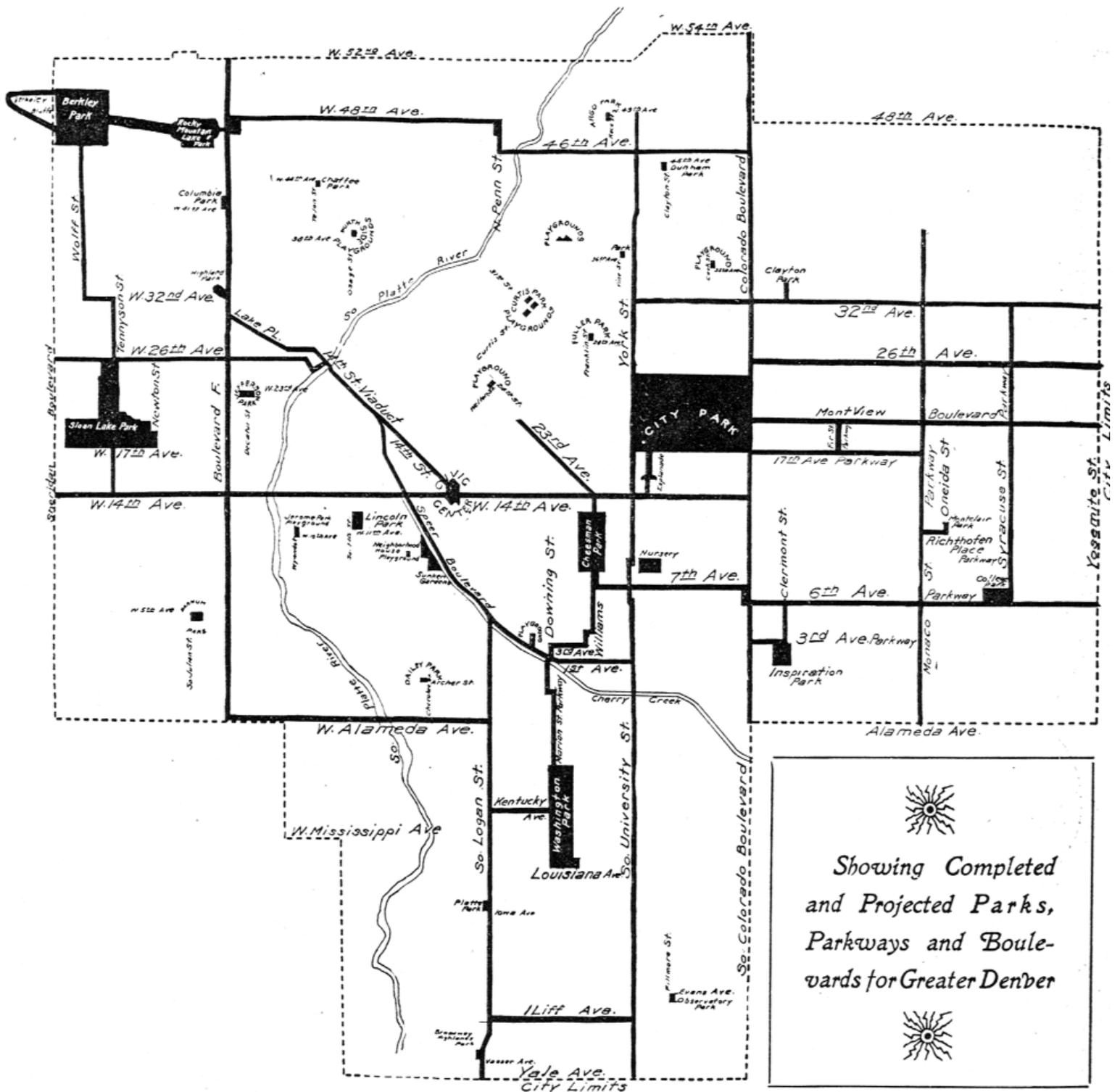
*Federal Boulevard, circa 1890-1910,
courtesy Western History Collection, Denver Public Library*

System planning continued and in 1878 Mayor Robert Sopris' presented his vision, the Sopris/Lee Plan, which proposed two city parks located equidistant to the east and the west from the state capital and connected by a shaded tree-lined boulevard. The 'east park' City Park, moved forward, but the 'west park' Sloan's Lake Park, remained a dream for years and the connecting boulevard was never built.

Montview Boulevard also was built during this time, laid out in 1892, developed as a parkway in 1902 when the roadway and its adjacent 'parking strips' (tree lawns) were built and completed in 1911, when trees and water lines were installed.

In 1894, Dr. John Evans prepared the city's first comprehensive city-wide *Park and Parkway* system plan, years before Mayor Speer's work, with the assistance of Edward Rollandet, Chief Draftsman for Denver's Department of Public Works. Dr. Evans proposed ringing the city with four major public parks, each with a significant water feature, located well beyond the then city limits and linked by a grid of east-west, north-south and diagonal parkways and boulevards. Dr. Evans' proposal gained approval by City Council but languished amidst opposition and controversy. However, acquisition for park land continued and Evans' vision resurfaced in subsequent plans.

Designated Parkways and Boulevards



The 1909 Adopted Plan - 1909 "Completed and Projected Parks, Parkways, and Boulevards"
 courtesy Municipal Facts, October 1909, Western History Collection, Denver Public Library

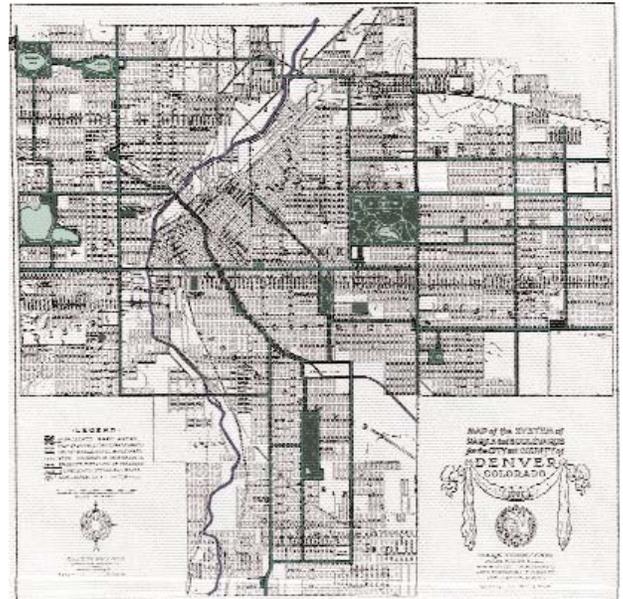
Civic Renaissance - 1905 to 1918

In 1905, Mayor Speer, who took office in 1902 embarked upon his campaign to beautify Denver by creating an Arts Commission and bringing Charles Mulford Robinson, a leading planner and noted national 'civic expert,' to Denver. Robinson's 1906 report, "*The Development of Denver*," became the definitive plan for Denver's civic and ". . . artistic improvement." It included proposals to transform the center of the city into "*a great Civic Center*" and to organize the city through a series of tree-lined boulevards to create an interconnected network of improved parks and new places for play.

In 1907 landscape architect George Kessler from Kansas City was commissioned to plan a city-wide system of parks connected by parkways and boulevards. Kessler's *1907 Map of the System of Parks and Boulevards* closely followed Robinson's vision, identifying 'double drive parkways' and 'single drive boulevards' to link parks and civic spaces all across the city. By 1909, a parkway and boulevard plan, the *1909 Adopted Plan*, was adopted by the city and was published in the October 9, 1909 edition of Denver Municipal Facts. Although there were a few key differences, most notably the addition of parkways such as 3rd and 4th Avenues in Mayor Speers's neighborhood, the *1909 Adopted Plan* closely resembled Kessler's. Today's system closely resembles the *1909 Adopted Plan* although as many as nine parkways, primarily those in the western portion of the city, were never developed including West 48th/46th Avenue at Denver's northern boundary and West 26th Avenue from Sloan's Lake Park to the South Platte River.

Design for many parkways and boulevards began as early as 1906 with city engineers, along with consultants such as George Kessler, working on plans for Speer Boulevard and other streets. Influential businessmen, city engineers, architects, artists and landscape architects worked side by side to design, fund and build the parkways and boulevards. Funds were secured for improvements through a series of 'improvement districts' that combined private property assessments with city funding that would be used over the next 10 years to build the city's infrastructure. Between 1906 and 1918, more than six districts funded roadway grading, surfacing, curbing, walks, plantings and water line installation.

Construction for portions of Speer Boulevard began in 1906 with completion by 1908. By 1906, land was

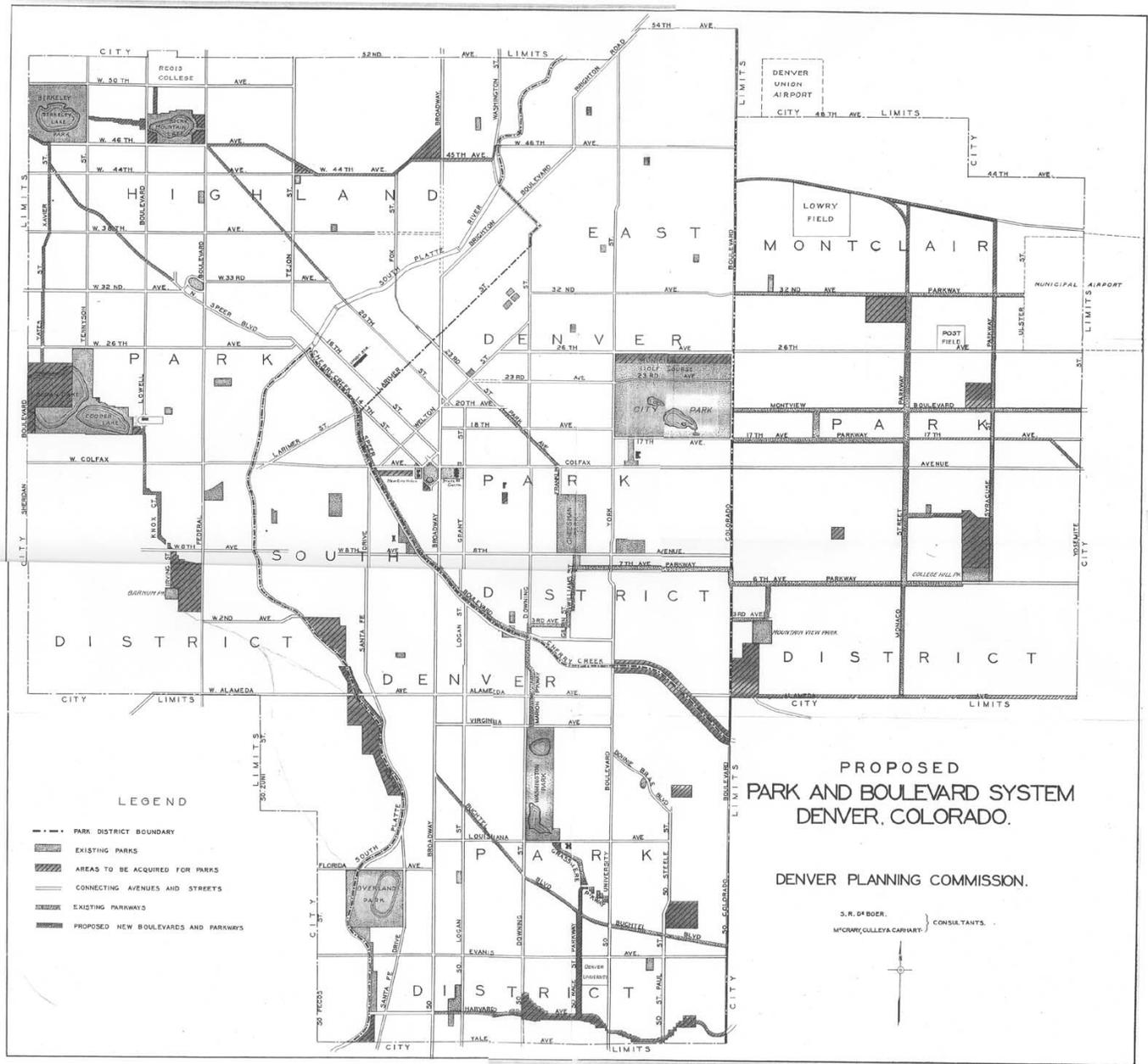


George Kessler's 1907 Map of the System of Parks and Boulevards. Courtesy Western History Collection, Denver Public Library

acquired for the City Park Esplanade and by 1907 portions of the parkway were in place. In 1909 the Montclair Parkway Suburban Improvement District No. 1 was established to fund improvements east of Colorado Boulevard, extending as far north as 22nd Avenue and to East 7th Avenue on the south. The largest of all the 'improvement districts,' it was responsible for the improvement of East 17th Avenue Parkway, Monaco Street Parkway from East 1st Avenue to Montview Boulevard, East Sixth Avenue Parkway, Fir (now known as Forest) Street Parkway and Clermont Street Parkway. In 1910, in the southern part of the city, the South Denver Improvement District No. 8, provided funds for construction of South Marion Street Parkway.

Construction of the parkways of the Denver Country Club neighborhood, including Franklin, Gilpin and High Streets as designed by William E. Fisher in occurred in 1903. In 1912, the Williams Street Improvement District provided funds for construction and improvements to Gilpin Street, East 4th Avenue, Downing Street, Williams Street and East 3rd Avenue.

Designated Parkways and Boulevards



Evans Rollendet Plan, 1894. Courtesy Denver Public Library, Western History Collection

Parkway and Boulevard System - 1927 to 1970

As evidenced by the first two significant periods of development, the creation of the parkway and boulevard system occurred in spurts with planning and construction offset by periods of little activity and even years where no work occurred. This pattern would continue until the 1970s but with notable differences. After 1918, there were extended periods when no development occurred. The system grew with the addition of eight more parkways and boulevards but many were built one or two at a time, and unlike their earlier predecessors, they had a wider variety of styles and several were not planted until the 1990s.

After 1918, intense construction of parkways and boulevards slowed considerably until the mid-1920s. However, home building that had already begun around 1909, continued into the late 1940s and early 1950s with the biggest boom occurring between 1928 and 1941. The design and siting of these original homes follow the basic tenets of the city's vision that the parkways and boulevards would serve as grand avenues. They were set away from the street at a generous distance and along a consistent line. The sizes of the homes were similar to each other along individual parkways and boulevards but differed from street to street. The front facades usually faced the parkway or boulevard and early landscaping accentuated the openness of the park-like setting.

City planning as it related to parkways/boulevards seemed to take a hiatus in the years immediately following 1918. But by 1925, Denver had initiated its zoning ordinance and by 1926 new city-wide plans were once again moving to the forefront. Between 1927 and 1929, S.R. DeBoer, along with the landscape architectural firm of McCrary, Culley and Carhart prepared a comprehensive plan for the city, which included a street and transportation plan "Major Street Plan," recommending new streets and street widening and a "Proposed Park and Boulevard System" plan. Adopted by the city in 1929, the two plans were instrumental in setting a direction for city streets which would result in positive changes and detrimental impacts. Most notable would be classifying previously described parkways and boulevards to 'existing major streets,' instead of as 'connecting avenues and streets.' These included Speer Boulevard, Colorado Boulevard, Federal Boulevard and Park Avenue. DeBoer's comprehensive plan was the first to

recommend that the parkways and boulevards be further protected by eliminating truck traffic from these routes as they were intended only as park connections that would carry light automobile traffic.

During the 1930s only three parkway/boulevards were added to the system - Bonnie Brae Boulevard, Buchtel Boulevard and Cherry Creek Drive. Each is distinctively different from the others.

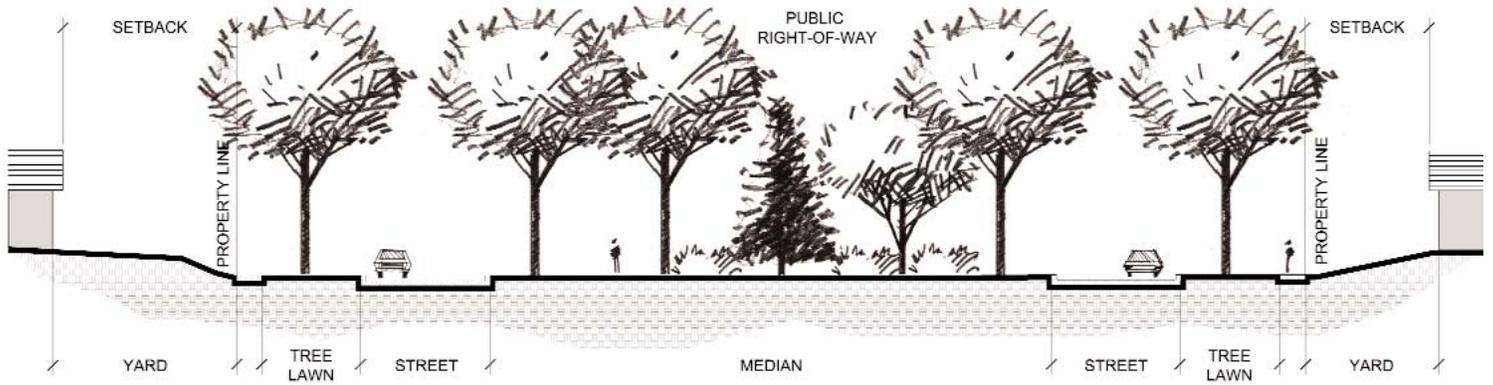
By World War II little work was occurring on the parkway and boulevard system. Only Hale Parkway and South Irving Street Parkway were built in the mid-1940s but were not planted. In 1950 the Parkway and Boulevard Ordinance was created to provide a high level of protection allowing for the **designation** of certain streets as parkways and boulevards; requiring that "*every home . . . face the parkway or boulevard;*" formalizing existing building setbacks as a requirement; and allowing the Manager of Parks and Recreation to regulate the setback distances, providing the manager with review authority.

In 1955, four special park districts (Highlands, East Denver, Montclair, and South Denver) that were originally initiated in the early 1900s by Mayor Speer were combined by City Ordinance No. 84 into one entity - the Denver Parks and Recreation Department. Independent of the Department of Public Works, this department was charged with managing the city's parks and designated (by city ordinance) parkways and boulevards.

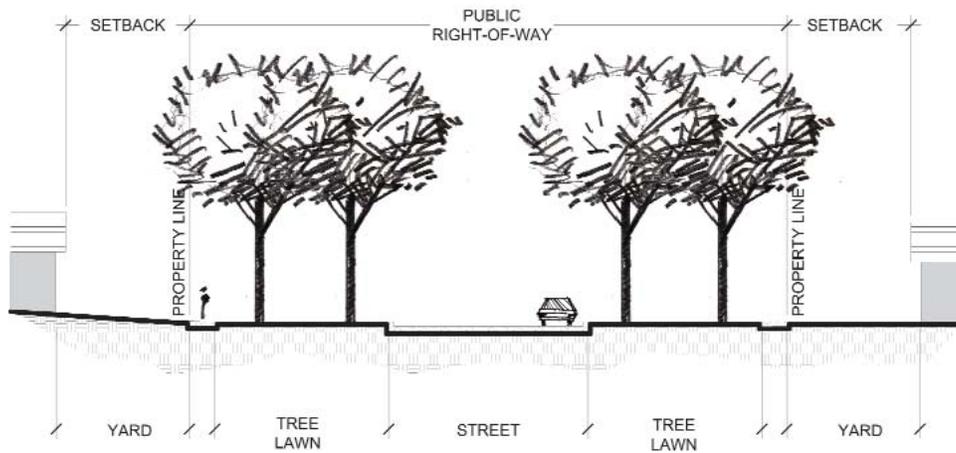
In 1965 the Denver Planning Office undertook a study for the improvement of the parkways and boulevards. *The Parkway Plan*, documented the development of the parkway and boulevard system, inventoried the designated parkways and boulevards including traffic volumes, and provided recommendations for the system and individual streets. Founded on 'environmental beautification' and organized by the function of the street, the plan focused on proposals for landscaping those designated parkways and boulevards that were not planted and on adding (and improving) new streets to the designated system. A key recommendation was to define acceptable levels of traffic for each of the parkways and boulevards, including acceptable speed limits and restrictions.

With the completion of East Alameda Avenue in the early 1970s the system of designated parkways and boulevards as it exists today was complete.

Designated Parkways and Boulevards



Components of a Parkway or Boulevard with a median.



Components of a Parkway or Boulevard without a median.

Section 3. General Design Guidelines

A.1 Preservation of Character

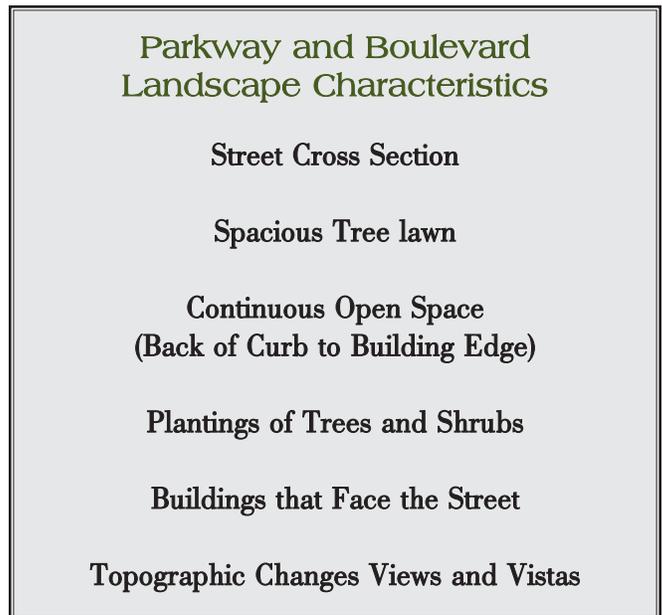
Denver's designated parkways and boulevards are broad, landscaped streets that have a park-like setting. Designed individually to create a city-wide system of wide tree-lined streets that link Denver's important civic spaces, the parkways/boulevards share similar characteristic features.

The character of the parkways and boulevards is defined by elements that are located within both the public right-of-way and the yard of the adjacent private property. Character defining features include the entire street cross section; a spacious tree lawn on both sides of the street; a continuous open space from the back of the curb to the building edge; and plantings of trees and shrubs.

Buildings traditionally face the parkway/boulevard and are setback from the street by the regulations set by the Parkway Ordinance to create a distinctive consistent edge

Many of Denver's parkways and boulevards were aligned and spatially organized to allow for views and vistas of the Rocky Mountains to the west and to create distinctive views to important civic spaces such as Denver's regional parks.

- a. Respect and preserve the historic or original design of the parkway or boulevard. Components of the original design usually include the street cross section, tree lawn, median (if appropriate), adjacent street pattern, building setback and private yard, building siting, topography and plantings.
- b. Respect and preserve the system of parkways and boulevards throughout the city.
- c. Maintain the design style of the parkway or boulevard. Do not attempt to create a nostalgic or a false sense of history by adding elements or features or make it appear older than it is.



d. Maintain and preserve views and vistas as character defining features.

e. Before undertaking any alteration or modifications, inventory and assess the extent of original or historic features and their relationships to one another to determine the compatibility of any proposed changes.

f. A clear understanding of the intent of the original design and the extent to which it was implemented is essential in making decisions on how to change or modify a parkway or boulevard.

A.2 Preservation of Original Features

The original materials and features of each parkway and boulevard as well as the spaces created by the building setbacks and their siting contribute to the character of each parkway and boulevard and should be preserved and respected.

The character-defining features of the parkway or boulevard should be preserved using the simplest methods possible. For historic parkways and boulevards, it is important that they retain a high percentage of original features to retain their historical integrity.

- a. Preserve features and elements that are part of the original parkway or boulevard construction, including the street, tree lawn, walks, curb and gutter, vegetation, structures, and monuments.
- b. Preserve the spaces and relationships that are part of the original or historic parkway or boulevard construction including the continuous open space created by the building setback.
- c. Avoid removing or altering original materials and features.
- d. If restoration or repair of original features is not possible, replace original features with elements that are of a similar form, scale, pattern and material as the original.



East 7th Avenue Parkway

A.3 Street Cross Section and Topography

The street cross section is one of the most characteristic features of a parkway or boulevard. As such, incremental changes such as road widening can cause broad detrimental impacts. In some cases, the accumulation of changes overtime has resulted in the eventual loss of the parkway or boulevard character such as with Colorado Boulevard and South Federal Boulevard.

Since each parkway and boulevard was designed individually, each has its own distinct street cross section. Many have medians that divide travel lanes and others have spacious tree lawns that flank both edges of a wide street. These individual components create the parkway or boulevard's street cross section and are a character-defining element.

The shape, vertical changes, elevation and contour of most of the parkways and boulevards are generally very subtle and will require some analysis to fully understand their character.

- a. Preserve the original road cross section including its patterning, arrangement, and the relationships between its components.
- b. Retain the original width and arrangement of the roadway, individual travel lanes, the median, and the tree lawn where they exist.
- c. Preserve and restore the original materials associated with the roadway such as the original curb and gutter where it exists.
- d. Patch, piece-in, splice or consolidate or otherwise upgrade the existing materials using recognized preservation methods whenever possible, rather than remove the element.
- e. Inventory and assess the patterning, arrangement and relationships to determine the compatibility of any proposed changes. Understand the intent of the original design and the extent to which it was implemented to determine compatibility of changes.



Distinctive topographic change along Bonnie Brae Boulevard.

- f. Preserve existing topographical changes along historic parkways and boulevards, including topography associated with the median, tree lawn and within the building setback along front, side and rear yards.
- g. Preserve the form and shape of individual spaces and their associated vertical elements.

A.4 Tree Lawns

Tree lawns are part of the public right-of-way and are one of the most easily recognizable and character-defining features of a parkway or boulevard. Historically known as ‘parkings’, tree lawns are the space between the curb and the sidewalk that has traditionally been planted with turf grasses and linear rows of street trees. As a distinctive and defining characteristic of the parkways and boulevards, tree lawns should be preserved.

- a. Preserve the original or historic tree lawn including the patterning, arrangement and relationship between the street and the adjacent property.
- b. Preserve the consistent, soft planted appearance of the tree lawn. Appropriate turf and low growing grasses shall be used to maintain the continuous smooth appearance. The use of paving and other hardscape materials is inappropriate.
- c. Preserve healthy original or historic trees where they exist. Refer to Guideline A.9. for the treatment of trees and shrub plantings.



The tree lawn along University Boulevard

A.5 Sidewalks

Tree lawns with an adjacent sidewalk are an important characteristic of most of the parkways and boulevards and as such they should be preserved where they exist. While sidewalks, and the locations and widths of sidewalks, may appear to be the same for every parkway and boulevard, each has its own individual characteristics. Traditionally, the sidewalk was detached and located near the outer edge of the public right-of-way. Newer parkways and boulevards such as South Irving Street have modified this tradition by providing a narrower sidewalk located immediately adjacent to the street. Both patterns are character defining.

- a. Preserve original or historic sidewalks, including those of concrete and sandstone, where they exist.
- b. Maintain sidewalks in good repair. For historic sidewalks, patch, piece-in, splice, consolidate or otherwise upgrade the existing materials using recognized preservation methods whenever possible, rather than removing the sidewalk.
- c. Replace with in-kind materials - i.e., concrete for concrete, sandstone for sandstone - retaining the original sidewalk width. Where a wider sidewalk than that which existed originally is required, maintain the pattern, alignment and location of the original sidewalk. For example, if the original sidewalk is linear do not curve the new sidewalk.
- d. Stamped or colored concrete is not an appropriate replacement material, but may be used for accessible ramps. Locate accessible ramps to follow the pattern and alignment of the individual parkway or boulevard.
- e. New sidewalks may be added within the public right-of-way but they must be sited to be compatible with the patterns of the parkway or boulevard. For example, a new sidewalk can be added to East 6th Avenue Parkway if it is located near the outer edge of the public right-of-way.



The sidewalk and tree lawn along East 17th Avenue Parkway.



Attached sidewalks along the northern segment of Monaco Street Parkway are a character-defining feature.



Williams Street building setback.

A.6 Parkway Building Setbacks and Siting Buildings and Structures

The parkway building setbacks are a critical component of all of Denver's designated parkways and boulevards and are particularly important to those that are also historically significant. The parkway building setbacks are established by the Parkway and Boulevard Ordinance.

Traditionally, the parkway building setback has extended the green, open character of the parkway or boulevard to the building edge. The result is a visual open space that expands the perception of the parkway/boulevard as a 'green' roadway.

Setting the buildings from the curb at a generous and consistent distance dates from the initial construction of the parkways and boulevards and their adjacent buildings. The setback has traditionally created a strong edge of aligned structures that visually enclose the parkway or boulevard. Keeping the yard within the setback clear of elements such as solid fences, walls and 'barrier' landscaping is important to preserving the open park-like character of the parkway and boulevard.

- a. Preserve the front building setback for each parkway and boulevard as defined by the Parkway Ordinance. New buildings, structures and additions shall be sited to respect and reinforce the front building setback.
- b. Proposals for siting buildings, structures and additions should be made and reviewed in regards to the impact they will have on the entire length of the parkway or boulevard.
- c. Respect back building setbacks where feasible for each parkway and boulevard as defined by the Parkway Ordinance.
- d. Solid vertical elements are not allowed within the parkway building setback as they disrupt the continuity of the open space solid vertical elements include any buildings, sheds, garages, solid fences, columns, walls, hedges or barrier type landscaping.

Excerpts from Article II. Parkway and Boulevard Ordinance

Sec. 49-17. Setback regulations.

(a) Parkway and boulevards named in this article shall be subject to such setback regulation of structures as the manager of parks and recreation may file with the building and zoning department. . .

Sec. 49-18. Building to front on parkway or boulevard.

Every building shall either face the parkway or boulevard or one (1) of two (2) fronts shall face the parkway or boulevard. . .

- e. Refer to A.10 Fences and Structures for specific guidelines regarding proposed fences or other small scale structures.

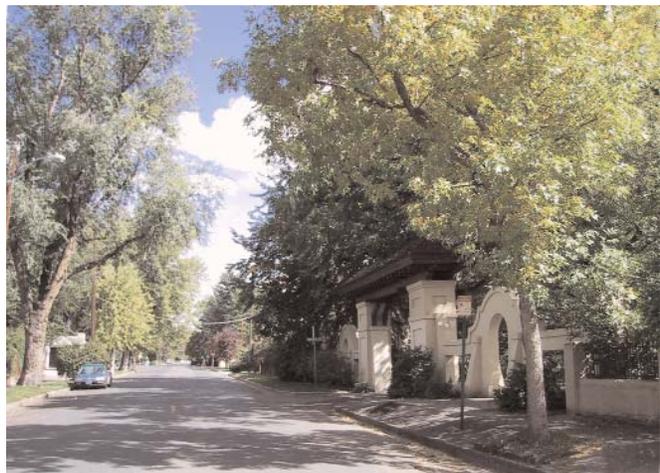
A.7 Monuments and Gateways

Original or historic monuments and gateways are character-defining features of specific parkways and boulevards such as those within the Denver Country Club on Franklin, Gilpin and High Streets. Each is constructed of distinctive materials and a design style that contributes to the significance of the parkway or boulevard and to its neighborhood.

- a. Preserve original or historic monuments and gateways and their settings including significant vegetation, lighting and other associated features.
- b. Maintain monuments in good repair. Patch, piece-in, consolidate or otherwise stabilize the existing materials using recognized preservation methods.
- c. Additional gateways or monuments should not be added to parkways/boulevards where historic or original monuments are character-defining features.
- d. Prune or trim adjacent vegetation. Protect adjacent vegetation that is integral to the setting of the monument or gateway.



Franklin Street



4th Avenue Parkway



Sullivan Gateway, City Park Esplanade

A.8 Signs

Existing signs are minimal on the parkways and boulevards and are generally those that are regulatory or informational. Regulatory signs are placed by the City and County of Denver primarily for traffic, schools, pedestrian and biking, safety and parking control. Informational signs include those placed by the City and County of Denver to identify city facilities such as the Denver Zoological Gardens, city parks, and those placed by private property owners to identify businesses. All signs along parkways and boulevards are subject to a sign setback and other requirements as defined by the Parkway and Boulevard Ordinance.

- a. Signs shall not be placed along or within the median of a parkway or boulevard.
- b. Minimize the number of regulatory signs along the parkway or boulevard. Ensure that the placement of new signs does not obscure visibility or interfere with significant views and vistas.
- c. Where new signs are necessary, poles shall be placed to respect the original or historic tree patterns of the individual parkway or boulevard. New poles shall be sited at an adequate distance from the curb to allow for car bumpers, door swings, and for access to driveways and alleys. For more detail on each parkway or boulevard refer to the [Assessment and Management Recommendations](#).
- d. The sign setback shall be followed for each parkway and boulevard as defined by the Parkway Ordinance
- e. Advertisements are not allowed.
- f. Signs on buildings shall meet the parkway and boulevard sign setback requirements. Where building and sign setbacks conflict, the more stringent shall apply.

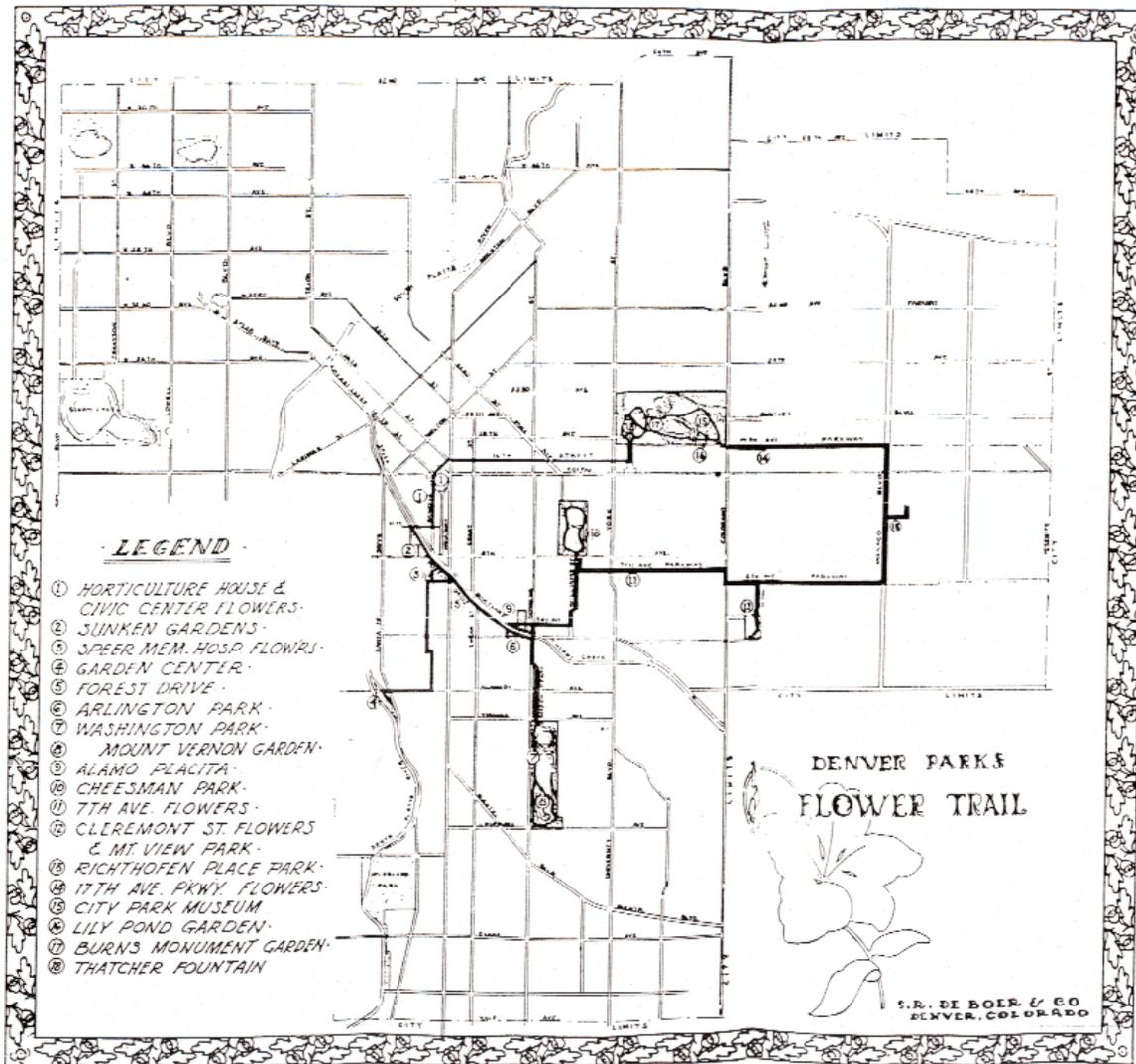
Excerpts from Article II. Parkway and Boulevard Ordinance

Sec. 46-2. Signs along parkways and boulevards.

It shall be unlawful to erect or remodel any ground sign or roof sign upon or along any boulevard or parkway, except in business or industrial districts.



Existing signs along Montview Boulevard.



Flower Trail Map

A.9 Vegetation

The most recognizable features of the parkways and boulevards are the linear street trees, groves of ever-green and deciduous trees and shrub massings that are all set in a horizontal plane of a low turf grass lawn.

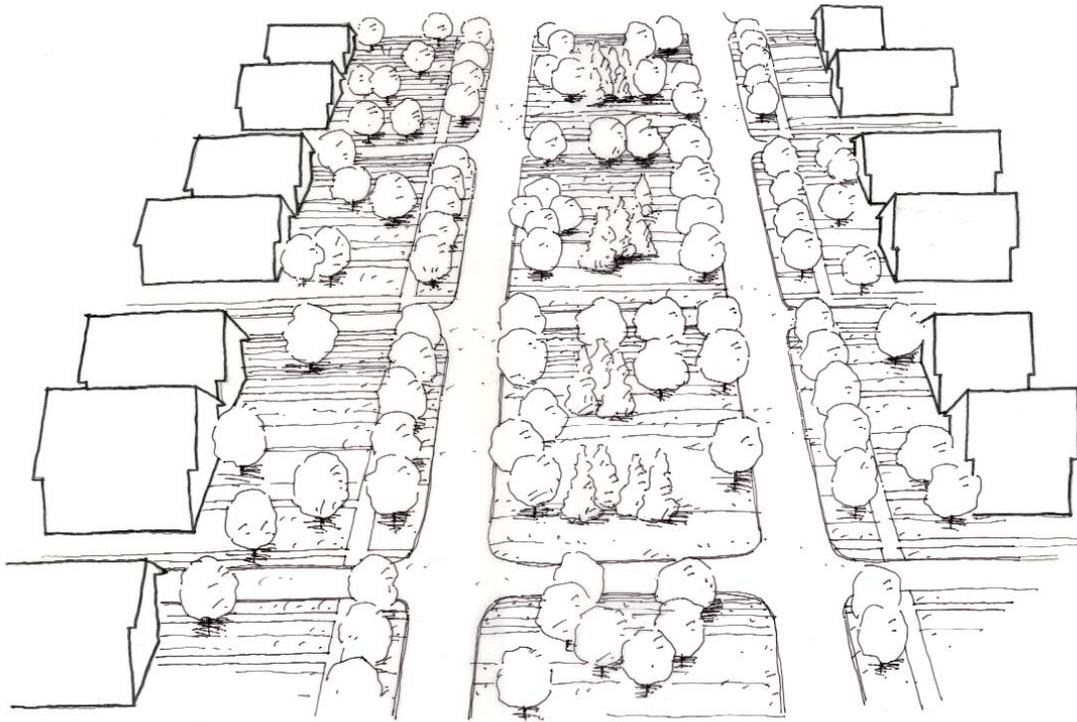
Each parkway and boulevard has its own individual arrangement of plantings that creates its character. Although the parkways and boulevards share similar features such as allees of deciduous trees, each has its own characteristic palette of trees and shrubs and its own unique arrangement and patterns.

- a. Preserve the original or historic plantings, including original material such as trees and shrubs. Preserve the characteristic patterns, arrangements and relationships of the original historic plantings.
- b. Protect original trees and shrubs where they exist. For example, protect the distinctive allees of American Elms on East 17th Avenue Parkway and the unique single row of Honeylocusts on West 46th Avenue Parkway.
- c. Plantings of in-fill trees and shrubs shall follow the original patterns, spacing and arrangement that characterize each parkway or boulevard rather than relying on the city's uniform standard for street tree plantings.
- d. Plantings of new trees and shrubs shall be the same species as the original with the same form, habit, texture and color. If the same species is not available, a similar species with similar characteristics, including height, form, texture and color can be planted. For example, a newer thornless variety of a Honeylocust tree could be used for West 46th Avenue Parkway and other parkways and boulevards where they are character defining features since the original trees had thorns.
- e. All vegetation shall be irrigated with an automatic underground irrigation system that is designed for low water use. A separate zone for tree and shrub irrigation should also be provided.

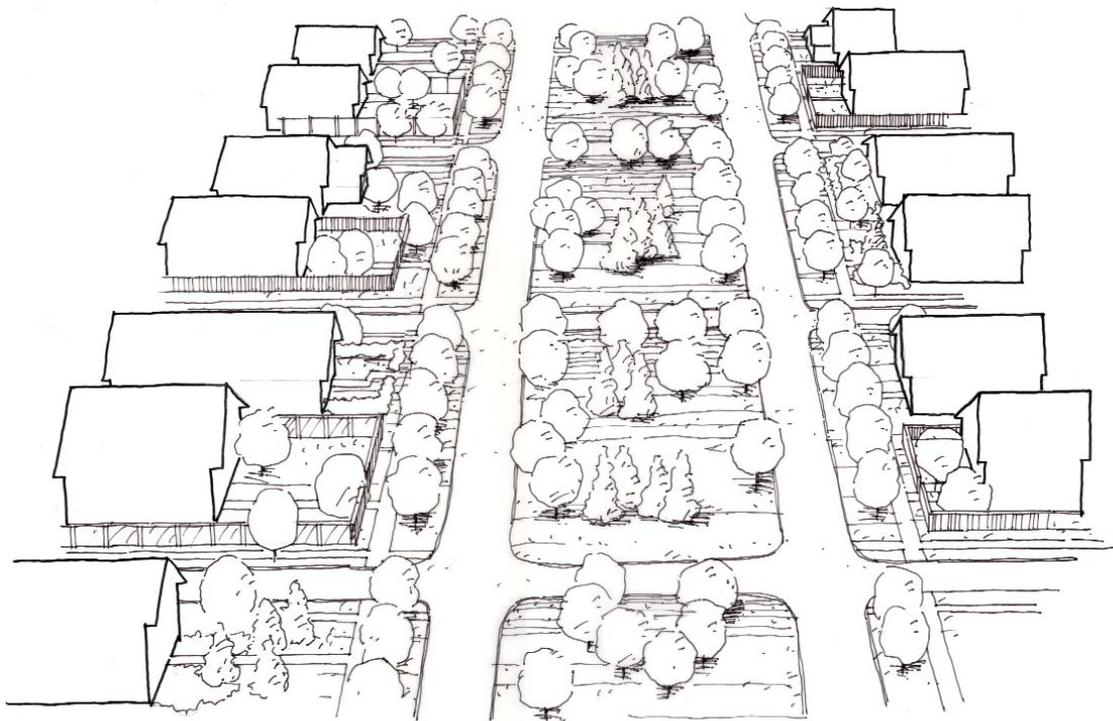


East 17th Avenue Parkway's distinctive allee of deciduous trees (American Elms)

- f. Plantings within the building setback shall be designed so they do not detract from the parkway or boulevard setting, allowing for the continuous open space to remain visible.
- g. Preserve original or historic flower beds, including the “*flower trail*” where they exist. Re-establish original flower beds when adequate documentation is available.
- h. Refer to the [Assessment and Management Recommendations](#) for descriptions of original planting patterns, species and arrangements.



Adhering to the building setback preserves the park-like setting of a parkway or boulevard.



The addition of fences, landscaping or building additions disrupts the continuous open space of the parkway or boulevard.

A.10 Fences and Small Scale Structures

The character of the designated parkways and boulevards is created by the expansive park-like setting that extends uninterrupted from building edge to building edge and continues for the entire length of the parkway or boulevard. Even small scale additions to the parkway building setback such as fences and landscaping, disrupt the continuity of the distinctive park-like setting.

- a. Preserve the continuous park-like setting by siting solid fences and other small scale structures to align with the parkway building setback as regulated by the Parkway and Boulevard Ordinance.
- b. Fences within the parkway building setback are not allowed. This requirement may be waived by the Manager of Denver Parks and Recreation in accordance with the Parkway and Boulevard Ordinance.
- c. If a new fence is approved, it shall be designed to be compatible in style and material with its adjacent building. It shall also be designed so that it does not detract from the parkway or boulevard setting, meaning that it shall be transparent to allow for a reading of the continuous open space that extends across the public right-of-way to the building edge.



Transparent Fence

A.11 Pedestrian Lighting

Pedestrian-scale lighting was an original feature of some of the designated parkways and boulevards. Both Speer Boulevard and the City Park Esplanade had similar light fixtures that were set within their tree lawns.

- a. Pedestrian lights that mimic or replicate the original or historic lights can be introduced to parkways and boulevards that historically had such.
- b. Place new pedestrian lighting to respect the original or historic tree patterns. Site the lighting fixtures according to original lighting patterns, i.e., maintain a similar distance from the curb.
- c. New pedestrian lighting shall be compatible with the character of the parkway or boulevard and contemporary in style.
- d. Place new pedestrian lights in alignment with existing tree patterns. At a minimum, site new poles at an adequate distance from the curb to allow for car bumpers, door swings and access to driveways and alleys.
- e. Pedestrian historic light fixtures that evoke a nostalgic or false sense of history such as those that exist along Speer Boulevard shall not be installed lighting along historic or designated parkways or boulevards where the lighting did not exist originally.



Original pedestrian lighting along Speer Boulevard circa 1910-1920, courtesy Denver Public Library, Western History Collection



Pedestrian lights that re-establish the original style and pattern, such as these along Speer Boulevard, are acceptable.

A.12 Street Lighting

Traditionally street lights were not important designed components of the parkways and boulevards and were generally added at a later date. Where street lights are in place, the style and location of the lights are generally unobtrusive and are not considered to be character-defining features.

Adequate lighting illumination levels are important to the safety of the parkways and boulevards and are determined by the City and County of Denver according to the uses along the parkway/boulevard (residential vs. commercial) and according to the volume of automobile traffic. While the poles and lights are usually unobtrusive, transformers, cabinets and meters that are associated with the street lighting are too often sited in visually prominent locations and detract from the parkway and boulevard character.

- a. Continue the tradition of providing unobtrusive poles and fixtures where street lighting is necessary. Ensure that the placement and selection of fixtures do not impact the character of the parkway or boulevard during the day or at night.
- b. Poles and fixtures that evoke a nostalgic or false sense of history are inappropriate. If new street lighting is introduced, fixtures should be unobtrusive and contemporary in style.
- c. Place new poles and fixtures to respect the original or historic street and tree patterns along the parkway or boulevard while providing adequate illumination levels. Site new poles at an adequate distance from the curb to allow for car bumpers, door swings and access to driveways and alleys.
- d. Place transformers, cabinets, meters and other new lighting in locations where their installation will cause the least visual impact to the parkway or boulevard. Ensure that the placement does not have a negative impact on the physical or visual characteristics of an original or historic feature.
- e. Special effect lighting is not allowed.



Street Lighting along Monaco Street Parkway.

A.13 Driveways, Alleys and Parking

Each parkway and boulevard has its own characteristic street, alley and building patterning, and the majority have vehicular access from a side street or back alley. Very few of the parkways and boulevards have front driveways. The street, alley and building patterning of each parkway and boulevard are character-defining features and should be preserved.

The original street patterns are linear. Where parking was required it was generally accommodated as parallel parking that was located adjacent to the curb next to the tree lawn within the street cross section. In some instances, spaces have been notched into the tree lawn to provide parking.

- a. Preserve the original or historic street, alley and building patterning and arrangement of vehicular access to buildings along the parkway/boulevard.
- b. Preserve the existing form of street edges such as curb and gutter, radius, alley flares, etc. When new street improvements are necessary they should reflect the form, material, configuration and scale of the original construction.
- c. Do not install new driveways along parkways or boulevards where they do not or did not historically exist.
- d. Do not widen or expand existing driveways or alleys.
- e. Preserve the original street edge alignment. Do not modify the street edge to create parking.
- f. Do not expand existing parking spaces that are notched into the tree lawn. Reclaim these areas as the tree lawn and move the curb edge to align with the street when parking area is no longer needed or allowed.
- g. Parking lots, drives or required screening features are not allowed within the building setback.



Uninterrupted street edge on Marion Street Parkway.

A.14 Bus Stops

Bus stops and shelters were not usually designed as original components of the parkways and boulevards and most were added at a later date. The Regional Transportation District (RTD) and the City and County of Denver determine the locations of bus stops according to vehicular traffic patterns and place shelters at locations with high ridership. RTD only places benches as part of a shelter.

- a. Bus stops, benches or shelters shall not be placed along the median of a parkway or boulevard.
- b. In locations where two bus lines intersect shelters shall be placed on cross streets adjacent to the parkway or boulevard and not on the parkway or boulevard itself.
- c. Advertisements are not allowed.



The siting of the RTD bus stop in the center of the median along Monaco parkway detracts from its character. Shelters, stops and benches should be located so that they do not detract from the parkway or boulevard or disrupt views or vistas.

A.15 Utility Boxes, Mailboxes and Underground Utilities

Utility boxes, mailboxes and underground utilities are often installed along a parkway or boulevard to provide services to adjacent neighborhoods. Historically, utilities were located under the street to minimize disruptions to the neighborhood and to be visually unobtrusive.

- a. Continue the tradition of providing utilities in an unobtrusive manner.
- b. Place utility boxes, mailboxes and other above ground utilities where their installation will cause the least visual impact to the parkway or boulevard. Ensure that the placement does not have a negative impact on the physical or visual characteristics of an original or historic feature.
- c. Place new underground utilities within the street. Do not locate utilities under sidewalks or within the tree lawn or other areas where they might interfere with existing trees or preclude future plantings.
- d. Place new utility boxes, mailboxes and other above ground utilities to respect the original or historic street and planting patterns along the parkway or boulevard.
- e. Site new utilities at an adequate distance from the curb to allow for car bumpers, door swings, and access to driveways and alleys.



The siting of this existing utility on East 17th Avenue Parkway detract from its character.

Appendix



City and County of Denver

November 2005

ARTICLE II. PARKWAYS AND BOULEVARDS

Sec. 49-17. Setback regulations.

(a) **Parkways** and boulevards named in this article shall be subject to such **setback** regulation of structures as the manager of parks and recreation may file with the building and zoning departments; provided, however, that such **setback** regulations shall not apply to police stations and provided further that where existing development of a particular block contradicts the intent of such **setback** regulations, permits may be approved consistent with the **setback** of existing structures on that block.

(b) The manager of parks and recreation may waive the **setback** requirements for fences and walls in excess of four (4) feet in height where it is found that the proposed fence or wall would be located on a **parkway** or boulevard where the impact of traffic and noise from said **parkway** or boulevard interferes with the reasonable use and enjoyment of the subject property and the fence or wall will not detract from the boulevard or **parkway** setting or the use of adjacent property. No waiver may be granted unless the proposed fence or wall has been approved by the department of zoning administration.

(Code 1950, § 621.2; Ord. No. 289-83, § 1, 5-16-83)

Sec. 49-16. Designation.

The following are designated as **parkways** and boulevards:

- (1) East Alameda Avenue--Steele Street to Quebec Street;
- (2) Berkeley Cutoff--West 46th Avenue at Yates Street to Sheridan Boulevard;
- (3) Bonnie Brae Boulevard--University Boulevard at Exposition to Mississippi Avenue at Steele;
- (4) Buchtel Boulevard--Clarkson Street to Colorado Boulevard;
- (5) City Park Esplanade--Colfax to East 17th Avenue;
- (6) Cherry Creek Drive (north and south sides)--University Boulevard to Quebec Street;
- (7) Clermont Street **Parkway**--East 3rd Avenue to East 6th Avenue;

- (8) Colorado Boulevard--South city limits to Dartmouth Avenue; Valley Highway to 44th Avenue;
- (9) Downing Street **Parkway**--East 3rd Avenue to East Bayaud Avenue;
- (10) Federal Boulevard--South city limits to 25th Avenue; 26th Avenue to north city limits;
- (11) Forest Street **Parkway**--East 17th Avenue to Montview Boulevard;
- (12) Hale **Parkway**--East 12th Avenue at Colorado Boulevard to East 8th Avenue at Grape Street;
- (13) South Irving Street **Parkway**--West Evans Avenue to West Jewell Avenue;
- (14) Joséphine Street--Cherry Creek to 2nd Avenue;

(15) Monaco Street **Parkway**--East 40th Avenue to Quincy Avenue except that portion of the west side of Monaco Street **Parkway** between the Highline Canal and Hampden Avenue in Arapahoe County;

(16) South Marion Street **Parkway**--East Bayaud Avenue at Downing, to Marion Street to East Virginia;

(17) Montview Boulevard--Colorado Boulevard to Syracuse Street;

(18) Park Avenue--East Colfax to East 20th Avenue;

(19) Richthofen Place **Parkway**--Monaco **Parkway** to Oneida Street;

(20) Speer Boulevard (north and south to Cherry Creek)--Federal Boulevard to Downing Street;

(21) University Boulevard--South city limits to Yale Avenue; Iowa Avenue to 6th Avenue;

(22) Williams, 3rd Avenue **Parkways**, and Country Club Islands--Williams Street from East 8th Avenue to East 4th Avenue; Fourth Avenue from Williams Street to Gilpin Street; Gilpin Street from East 4th Avenue to East 3rd Avenue; 3rd Avenue from Gilpin Street to Downing Street; Downing Street from East 3rd Avenue to Speer Boulevard; Franklin Street; 1st to 4th Avenue; Gilpin Street, 1st to 4th Avenue; High Street, 1st to 4th Avenue;

(23) East 1st Avenue--Downing Street to University Boulevard;

(24) East 3rd Avenue **Parkway**--Colorado Boulevard to Clermont Street;

(25) East 6th Avenue **Parkway**--Colorado Boulevard to Quebec Street;

(26) East 7th Avenue **Parkway**--Williams Street to Colorado Boulevard;

(27) West 14th Avenue **Parkway**--Bannock Street to Broadway;

(28) East 17th Avenue **Parkway**--Colorado Boulevard to Monaco **Parkway**;

(29) East 26th Avenue **Parkway**--York Street to Colorado Boulevard;

(30) East 32nd Avenue **Parkway**--Elizabeth Street to Quebec Street;

(31) West 46th Avenue--Grove Street to Sheridan Boulevard.

(Code 1950, § 621.1; Ord. No. 256-85, § 1, 5-13-85)

Sec. 49-18. Building to front on parkway or boulevard.

Every building shall either face the **parkway** or boulevard or one (1) of two (2) fronts shall face the **parkway** or boulevard; provided, however, where existing development or municipally approved platting contradicts the intent of this provision permits may be approved consistent with such existing development or platting.

(Code 1950, § 622.1)

Secs. 49-19--49-35. Reserved.

Sec. 46-2. Signs along parkways and boulevards.

It shall be unlawful to erect or remodel any ground sign or roof sign upon or along any boulevard or **parkway**, except in business or industrial districts.

(Code 1950, § 622.8)

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Bottom of Form

Sec. 49-510. News racks in parks or parkways.

The manager of parks and recreation shall have authority as defined in section 39-2 to adopt rules and regulations for news rack placement in parks and **parkways**.

(Ord. No. 918-01, § 1, 10-29-01)

Top of Form

Bottom of Form

Sec. 49-17. Setback regulations.

(a) **Parkways** and boulevards named in this article shall be subject to such setback regulation of structures as the manager of parks and recreation may file with the building and zoning departments; provided, however, that such setback regulations shall not apply to police stations and provided further that where existing development of a particular block contradicts the intent of such setback regulations, permits may be approved consistent with the setback of existing structures on that block.

(b) The manager of parks and recreation may waive the setback requirements for fences and walls in excess of four (4) feet in height where it is found that the proposed fence or wall would be located on a **parkway** or boulevard where the impact of traffic and noise from said **parkway** or boulevard interferes with the reasonable use and enjoyment of the subject property and the fence or wall will not detract from the boulevard or **parkway** setting or the use of adjacent property. No waiver may be granted unless the proposed fence or wall has been approved by the department of zoning administration.

(Code 1950, § 621.2; Ord. No. 289-83, § 1, 5-16-83)

**Sec. 39-6. Destruction of park property
unlawful.**

(a) It shall be unlawful for any person, other than authorized personnel, to cut, mark, remove, break or climb upon or in any way injure, damage or deface the trees, shrubs, plants, turf or any of the buildings, fences, bridges, monuments, fountains, back stops, goal posts, coin meters or other structures or property within or upon any park, **parkway**, mountain park or other recreational facility.

(b) It shall be unlawful for any person, other than authorized personnel, to pick or remove any vegetation in any park, **parkway**, mountain park or other recreational facility, or to go or enter areas of any park, **parkway**, mountain park or other recreational facility which are posted for no entry or no trespass.

(Ord. No. 1041-96, § 1, 12-2-96)

Appendix B - Parkway Building Setbacks

| Parkway | Building Setback |
|------------------------------|---|
| East Alameda Avenue | - Steel Street (3200) to Colorado Boulevard (4000): 20 feet both sides (fences excepted) - Colorado Boulevard (4000) to Monaco Street Parkway (6500): 12 feet both sides (fences or walls excepted) - Monaco Street Parkway (6500) to Havana Street (10500): 12 feet both sides (fences or walls excepted) |
| Bonnie Brae Boulevard | - 20 feet both sides |
| Buchtel Boulevard | None |
| Cherry Creek Drive | - University Boulevard (2400) to Holly Street (5600): 20 feet both sides - Holly Street (5600) to Quebec Street (7300): 20 feet both sides |
| City Park Esplanade | - 35 feet both sides |
| Clermont Street Parkway | - 30 feet both sides |
| Colorado Boulevard | - Hampden Avenue (3500 S.) to 44th Avenue (4400): 20 feet both sides <u>Except</u> - Dartmouth Avenue (3100 S.) to Florida Avenue (1500 S.): both sides exempt - Florida Avenue (1500 S.) to Cedar Avenue (200 S.): 10 feet both sides (fences and walls excepted) - 7th Avenue (700) to 9th Avenue (900): 10 feet both sides - 14th Avenue (1400) to 16th Avenue (1600): both sides exempt |
| Country Club Parkways | Franklin Street Parkway -25 feet both sides Gilpin Street Parkway -1st Avenue to 3rd Avenue: 25 feet both sides, -3rd Avenue to 4th Avenue: 35 feet both sides High Street Parkway -25 feet both sides 3rd Street Parkway -20 feet north sides 4th Street Parkway -20 feet north sides |
| Downing Street Parkway | -25 feet both sides |
| Federal Boulevard | - South City limits to Colfax Avenue (1500): 10feet both sides - Colfax Avenue (1500) to North City Limits: 20 feet both sides <u>Except:</u> - 25th to 26th Avenue: both sides exempt - 50th Avenue to 52nd Avenue: 10 feet both sides |
| Forest Street Parkway | - 25 feet both sides |
| Hale Parkway | -15 feet both sides |
| Irving Street Parkway | - 20 feet both sides |
| Marion Street Parkway | -25 feet both sides |
| Martin Luther King Boulevard | - Elizabeth Street (2600) to Dahlia Street (4800): 25 feet both sides - Dahlia Street (4800) to Quebec Street (7300): 20 feet both sides |
| Monaco Street Parkway | - Quincy Avenue (4200 S.) to Hampden Avenue (3500 S.): 20 feet both sides - Hampden Avenue (3500 S.) to Monaco Circle North (3100 S.): 25 feet both sides (fences excepted) - Monaco Circle North (3100 S.) to LaSalle Place (2500 S.): 25 feet both sides (except fences exceeding 48" in height shall be set back 10 feet) - LaSalle Place (2500 S.) to Dickenson Place (2450 S.): 5 feet east side, 25 feet west side - Dickenson Place (2450 S.) to Florida Avenue (1500 S.): 20 feet both sides (fences or walls excepted between Iliff and Evans Avenue) - Florida Avenue (1500 S.) to Exposition Ave (700 S.): 25 feet both sides - Exposition Ave (700 S.) to Dakota Ave. (400 S.): 20 feet both sides - Dakota Ave. (400 S.) to Alameda Ave. (300 S.): 25 feet west sides, 10 feet east side - Alameda Ave. (300 S.) to 1st Ave. (100): 25 feet both sides - 1st Ave. (100) to 6th Avenue (600): 35 feet west side, 25 feet east sides - 6th Avenue (600) to Montview Boulevard (2000): 35 feet west side, 20 feet east side - Montview Boulevard (2000) to 38th Ave (3800): 20 feet both sides (fences excepted) - 38th Ave (3800) to 40th Avenue (Smith Road 4000): no setback |
| Montview Boulevard | - Colorado Boulevard (3800) to Fairfax Street (5100): 50 feet north sides, 35 feet south side - Fairfax Street (5100) to Quebec Street (7300): 35 feet both sides - Quebec Street (7300) to Syracuse Street (7700): 25 feet both sides |
| Park Avenue | - 10 feet both sides |
| Richthofen Place Parkway | - 25 feet both sides |
| Speer Boulevard | - Federal Boulevard (3000 W.) to Elati Street (500 W.): no setback either side - Elati Street (500 W.) to Broadway (0): 10 feet SW side, no setback NE Side - Broadway (0) to Logan Street (400): 10 feet both sides - Logan Street (400) to Downing Street (1200): 20 feet both sides |
| University Boulevard | -South City Limits / Floyd Avenue (3300 S.): to 6th Avenue (600): 20t both sides <u>Except</u> - Yale Avenue (2700 S.) to Valley Hwy (1700 S.): both sides exempt - Valley Hwy (1700 S.) to Iowa Avenue (1600 S.): East side excepted - Ohio Avenue (800 S.) to Exposition Avenue (700 S.): 10t both sides - 3rd Avenue (300) to 6th Avenue (600): 20 feet both sides (fences excepted) |
| Williams Street Parkway | - 25 feet both sides |
| East 1st Avenue Parkway | - 25 feet both sides |
| East 3rd Avenue Parkway | - 35 feet both sides |
| East 6th Avenue Parkway | - 35 feet both sides |
| East 7th Avenue Parkway | - 20 feet both sides |
| West 14th Avenue Parkway | - 10 feet both sides |
| East 17th Avenue Parkway | - Colorado Boulevard (4000) to Monaco Street Parkway (6500): 35 feet north side - Colorado Boulevard (4000) to Dahlia Street (4800): 20 feet south side - Dahlia Street (4800) to Monaco Street Parkway (6500): 25 feet south side |
| East 26th Avenue Parkway | - York Street (3200) to Steel Street (3500): 15 feet both sides - Steel Street (3500) to Colorado Boulevard (4000): 25 feet both sides |
| West 46th Avenue Parkway | - 20 feet both sides |

Appendix B - Parkway Sign Setbacks

| Parkway | Sign Setback |
|------------------------------|---|
| East Alameda Avenue | -Steele Street (3200) to Colorado Boulevard (4000): 20 feet both sides -Colorado Boulevard (4000) to Quebec Street (7300): 12 feet both sides |
| Bonnie Brae Boulevard | -20 feet both sides |
| Buchtel Boulevard | -Logan Street (400) to Colorado Boulevard (4000): 20 feet both sides |
| Cherry Creek Drive | -University Boulevard (2400) to Quebec Street (7300): 20 feet both sides |
| City Park Esplanade | -35 feet both sides |
| Clermont Street Parkway | -30 feet both sides |
| Colorado Boulevard | -South City Limits (Hampden Avenue 3500 S.) to 44th Avenue (4400): 20 feet both sides <u>Except</u> -Dartmouth Avenue (3100 S.) to Asbury Avenue (2000 S.): both sides exempt -Asbury Avenue (2000 S.) to Alameda Avenue (300 S.): 5 feet both sides -Alameda Avenue (300 S.) to 7th Avenue (700): 10 feet both sides -7th Avenue (700) to 8th Avenue (800): 5 feet east side -7th Avenue (700) to 9th Avenue (800): 5 feet west side -14th Avenue (1400) to 16th Avenue (1600): both sides exempt -28th Avenue (2800) to 29th Avenue (2900): 5 feet both sides |
| Country Club Parkways | Franklin Street Parkway -25 feet both sides Gilpin Street Parkway -1st Avenue (100) to 3rd Avenue (300): 25 feet both sides -3rd Avenue (300) to 4th Avenue (400): 35 feet both sides High Street Parkway -25 feet both sides 3rd Street Parkway -20 feet north side 4th Street Parkway -20 feet north side |
| Downing Street Parkway | -25 feet both sides |
| Federal Boulevard | -South City Limits to 25th Avenue (2500): 5 feet both sides -26th Avenue (2600) to North City Limits: 5 feet both sides |
| Forest Street Parkway | -25 feet both sides |
| Hale Parkway | -15 feet both sides |
| Irving Street Parkway | -20 feet both sides |
| Marion Street Parkway | -25 feet both sides |
| Martin Luther King Boulevard | -Elizabeth Street (2600) to Dahlia Street (4800): 25 feet both sides -Dahlia Street (4800) to Quebec Street (7300): 20 feet both sides |
| Monaco Street Parkway | -Quincy (4300 S.) to Alameda Avenue (300 S.): 25 feet both sides -Alameda Avenue (300 S.) to 6th Avenue (600): 25 feet both sides -6th Avenue (6000) to Montview Boulevard (2000): 35 feet west side -6th Avenue (6000) to Montview Boulevard (2000): 20 feet east side -Montview Boulevard (2000) to 40th Avenue (4000): 20 feet both sides |
| Montview Boulevard | -Colorado Boulevard (4000) to Fairfax Street (5100) 50 feet both sides -Fairfax Street (5100) to Quebec Street (7300): 35 feet both sides -Colorado Boulevard (4000) to Quebec Street (7300): 35 feet both sides -Quebec Street (7300) to Syracuse Street (7700): 25 feet both sides |
| Park Avenue | -10 feet both sides |
| Richthofen Place Parkway | -25 feet both sides |
| Speer Boulevard | -Elati Street (500 W) to Logan street (400): 10 feet south side -Broadway (0) to Logan Street (400): 10 feet north side -Logan Street (400) to Downing Street (1200): 20 feet both sides |
| University Boulevard | -South City Limits/Floyd Avenue (3300 S.) to 6th Avenue (600): 20 feet both sides <u>Except</u> -East Yale Avenue (2700S.) to East Mexico Avenue (1700 S.): both sides exempt -Ohio Avenue (800 S.) to Exposition Avenue (700 S.) |
| Williams Street Parkway | -25 feet both sides |
| East 1st Avenue Parkway | -25 feet both sides |
| East 3rd Avenue Parkway | -25 feet both sides |
| East 6th Avenue Parkway | -35 feet both sides |
| East 7th Avenue Parkway | -20 feet both sides |
| West 14th Avenue Parkway | -10 feet both sides |
| East 17th Avenue Parkway | -Colorado Boulevard (4000) to Monaco Street Parkway (6500): 35 feet north side -Colorado Boulevard (400) to Dahlia Street (4800): 20 feet south side -Dahlia Street (4800) to Monaco Street Parkway (6500): 15 feet both sides |
| East 26th Avenue Parkway | -York Street (2300) to Steele Street *3200): 15 feet both sides -Steele Street (3200) to Colorado Boulevard (4000): 25 feet both sides |
| West 46th Avenue Parkway | -20 feet both sides |

Appendix C
Denver Landmark Historic Districts
with Parkways and Boulevards

Civic Center Historic District,
Ord. # 173
14th Avenue Parkway

Country Club Historic District,
Ord. # 448, D-18, 1990
3rd Avenue, 4th Avenue (south edge), Franklin
Street, Gilpin Street, High Street, Downing Street
(north of Speer, east side only), 1st Avenue (north
side only),

Denver City Beautiful Parkway,
Ord. # 132, D-29, 1997
East 6th Avenue Parkway, East 7th Avenue Parkway,
East 17th Avenue Parkway, City Park Esplanade,
Clermont Parkway, 3rd Avenue, 4th Avenue,
Downing Street, Monaco Parkway, Montview
Boulevard, Richthofen Place Parkway,
Speer Boulevard, South Marion Street Parkway,
University Boulevard, Williams Street Parkway

Driving Park Historic District,
Ord. # 90
Williams Street Parkway

Montclair Historic District,
Ord. # 487
Richthofen Place Parkway,

Park Avenue Historic District,
Ord. # 314, D-41, 2002
Park Avenue

Seventh Avenue Historic District,
Ord. # 292, D-2, 1993
East 7th Avenue Parkway,
Williams Street Parkway

Speer Boulevard Historic District,
Ord. # 604
Speer Boulevard

MUNDUS BISHOP DESIGN

Mundus Bishop Design, Inc. Landscape Architecture and Planning 1719 Boulder St., Denver, CO, 80211 Tel 303/477-5244 Fax 477-5146